

Article 19. Off-Street Vehicle & Bicycle Parking

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19.1 PURPOSE

The purpose of the off-street vehicle and bicycle parking regulations is to:

- A.** Manage parking to meet the intent and context of the various Place Types, utilizing tools such as minimum vehicle parking requirements and limits on the maximum amount of vehicle parking to be provided on-site.
- B.** Ensure safe and efficient parking facility design.
- C.** Provide bicycle parking and electric vehicle infrastructure to implement the City's goals of increasing the use of zero carbon energy sources and alternative modes of transportation.

19.2 VEHICLE PARKING SPACE REQUIREMENTS

A. Vehicle Parking Space Tier System

- 1. Table 19-1: Vehicle Parking Requirements are structured in a three-tier system as follows:
 - a.** Tier 1: A minimum number of off-street parking spaces are required. There are no off-street parking space maximums.
 - b.** Tier 2: A minimum number of off-street parking spaces are required. There are also off-street parking space maximums.
 - c.** Tier 3: A minimum number of off-street parking spaces are required for a limited number of uses and locations, but most uses do not have a minimum parking requirement. There are also off-street parking space maximums.
 - i.** Tier 3 required parking minimums may be reduced or eliminated upon Planning Director approval of a Parking Demand Management Assessment, as described in the Charlotte Streets Manual.
- 2. The tier that is applicable to each zoning district is identified in Table 19-1.
- 3. Where a cell is blank and shaded, no minimum and/or maximum parking is required.

B. Calculation of Vehicle Parking Spaces

Vehicle parking spaces are calculated by the principal use of the lot. When more than one principal use occupies the same lot, the number of spaces is the sum of the separate requirements for each principal use.

C. Minimum Off-Street Parking Space Requirements

- 1. When minimum off-street parking spaces are required by Table 19-1, the minimum vehicle parking requirements apply when any of the following occurs:
 - a.** New construction of a principal building.
 - b.** Expansion of an existing principal building resulting in a requirement of more than ten additional spaces.

c. Change of use or expansion of an existing use resulting in a requirement of more than ten additional spaces.

2. For existing parking facilities, the number of off-street vehicle spaces shall not be reduced below any minimum parking requirements of this Ordinance. If the number of such existing spaces is already less than the minimum required, it shall not be further reduced. However, if required streetscape improvements, curb relocation, or other requirements of this Ordinance cause the elimination of off-street parking spaces, these spaces need not be replaced.

D. Minimum Off-Street Parking Space Flexibilities

1. On-Street Parking Spaces

Existing on-street parking spaces abutting the property may be counted toward required minimum off-street parking spaces. New on-street parking spaces abutting the property may also count toward required minimum off-street parking.

a. Where on-street parking spaces are unmarked, the number of parking spaces is calculated by dividing the length of the on-street parking area abutting the property by the length required for a parallel parking space in the Charlotte Land Development Standards Manual (CLDSM).

b. Where on-street parking spaces are marked, each marked space counts as one required parking space, including any space where at least 50% of the length is abutting the property.

c. Spaces shall be accessible to the public 24 hours a day.

d. In the event that the city or state removes any such on-street parking that was allowed to count toward the minimum required, the existing use will not be required to make up the difference and the parking will not be made nonconforming.

2. Public Parking Reduction

Development sites located within 1,000 feet walking distance of public parking facilities may be granted a 20% reduction from the minimum parking requirement. Public parking facilities shall be owned or operated by a government agency or municipal service district, or developed as a public-private partnership, but do not include "park and ride" facilities for public transit.

3. Existing Structures or Tree Preservation

In the event that the required minimum parking spaces cannot be placed on the lot without the demolition of an existing structure or causing damage to significant trees on the site or in the public right-of-way, the Zoning Administrator may authorize up to a 25% reduction in the total number of minimum parking spaces required on the lot. In the case of significant trees, the Zoning Administrator shall consult with the Chief Urban Forester prior to authorization of the reduction.

a. The Zoning Administrator may issue such an authorization only upon the request of the applicant and only upon determining that the reduction in the number of required parking spaces will not unreasonably increase parking congestion along public streets or in parking areas located on nearby lots.

b. If such authorization is granted, the applicant shall not demolish or remove the existing structure or trees unless the full required number of off-street parking spaces are provided on the lot.

c. The Zoning Administrator may request measures be taken to help mitigate the reduction in parking. These mitigations may include, but are not limited to, pedestrian connections to public sidewalks and additional bicycle parking accommodations.

E. Parking Maximum Limitations for Existing Facilities

In the Transit Oriented Development Zoning Districts and the UC Zoning District, when at least 50% of the parking lot area is reconstructed, such parking lot shall meet any required parking maximums. Resealing, repaving, resurfacing, and/or re-striping of an existing parking lot are not considered reconstruction.

F. Spaces Exempt from Parking Maximums

Spaces reserved for the following are not included in calculating parking maximums:

1. Required accessible parking spaces in compliance with City, state, and federal standards.
2. EVSE-Installed electric vehicle charging stations (Section 19.3).
3. The following pick-up/drop-off spaces, which shall be marked as reserved:
 - a. Ride-hailing service vehicles.
 - b. Quick-commerce delivery vehicles.
 - c. Safe exchange zones, such as designated spaces in public areas for receipt of goods purchased online.
4. On-street parking spaces abutting the site.
5. On-site visitor spaces for multi-family dwellings to a maximum exception of ten spaces or 10% of the total number of on-site dwelling units (in spaces), whichever is greater. All such spaces shall be marked as available for visitor use.

G. Permissions to Exceed Parking Maximums

1. A parking maximum may be exceeded by up to 25% if one or more of the following are met:
 - a. 10% of the total number of spaces are provided for public use 24 hours a day and seven days a week.
 - b. 20% of the total number of spaces are provided for public use as shared spaces available from 8:00 a.m. to 6:00 p.m., Monday through Friday.
 - c. 20% of the total number of spaces are provided for public use as shared spaces available from 6:00 p.m. to 8:00 a.m., seven days a week.
2. When public use spaces are provided in order to exceed a parking maximum, the following apply:
 - a. When located within a parking structure, public use spaces shall be located within the first two floors of the structure.
 - b. Signage shall be provided that indicates the location of public use spaces.
 - c. Shared spaces that are not available 24 hours a day and seven days a week shall be clearly marked with the hours of availability for public use.
 - d. The facility may charge for the use of for public parking spaces.
3. Public transit facilities shall be exempt from parking maximums.

H. Alternative Parking Near Existing Transit Stations

Any property within one-half mile walking distance of an existing rapid transit station may use the Tier 3 parking standards, unless the property is located in a Neighborhood 1 Place Type. If Tier 3 parking standards are used, such standards shall be used in their entirety, including any applicable parking minimums and maximums.

I. Accessible Spaces

All parking facilities shall comply with City, state, and federal requirements for accessible parking spaces.

Table 19-1: Vehicle Parking Requirements

Uses	TIER 1 Neighborhood 1 Zoning Districts, N2-A, MHP, ML-1, ML-2, IC-1, OFC, OG Zoning Districts		TIER 2 N2-B, N2-C, IMU, IC-2, RC, NC, CAC-1, CG, CR Zoning Districts		TIER 3 CAC-2, TOD-UC, TOD-NC, TOD-CC, TOD-TR, RAC, UC, UE Zoning Districts	
	Minimum	Maximum <i>Tier 1 does not have a parking maximum</i>	Minimum	Maximum <i>Applies to both parking lots and parking structures</i>	Minimum <i>Applies only when within 400' of a Neighborhood 1 Place Type</i>	Maximum <i>Applies to both parking lots and parking structures</i>
Residential Uses						
RESIDENTIAL USE unless listed below	1/dwelling unit		1/dwelling unit	2 /dwelling unit		2/dwelling unit
Dormitory	1/2 dorm rooms		1/4 dorm rooms	1/dorm room		1/dorm room
Dwelling – Duplex	1.5/dwelling unit		1/dwelling unit	No limit on enclosed/garage spaces; 2 unenclosed/dwelling unit		No limit on enclosed/garage spaces; 2 unenclosed/dwelling unit
Dwelling - Live/Work	1/dwelling unit + 1/500sf GFA of commercial space		1/dwelling unit + 1/1,000sf GFA of commercial space	2/dwelling unit + 1/250sf GFA of commercial space		2/dwelling unit + 1/250sf GFA of commercial space
Dwelling - Manufactured Home	1/manufactured home		1/manufactured home	2/manufactured home		2/manufactured home
Dwelling – Multi-Family Attached When Units Not on Sublots <i>Also applies to residential component of mixed-use development</i>	1.5/dwelling unit; For senior living – 0.25/dwelling unit		1/dwelling unit; For senior living – 0.25/dwelling unit	1/bedroom/studio unit; For senior living – 0.75/dwelling unit	1/dwelling unit; For senior living – 0.25/dwelling unit	1/bedroom/studio unit; For senior living – 0.75/dwelling unit
Dwelling – Multi-Family Attached When Units on Sublots	1.5/dwelling unit		1/dwelling unit	No limit on enclosed/garage spaces; 2 unenclosed/dwelling unit		No limit on enclosed/garage spaces; 2 unenclosed/dwelling unit
Dwelling – Multi-Family Stacked <i>Also applies to residential component of mixed-use development</i>	1.5/dwelling unit; For senior living – 0.25/dwelling unit		1/dwelling unit; For senior living – 0.25/dwelling unit	1/bedroom/studio unit; For senior living – 0.75/dwelling unit	1/dwelling unit; For senior living – 0.25/dwelling unit	1/bedroom/studio unit; For senior living – 0.75/dwelling unit
Dwelling - Multi-Dwelling Development	<i>Based on dwelling types in development</i>		<i>Based on dwelling types in development</i>	<i>Based on dwelling types in development</i>		<i>Based on dwelling types in development</i>

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Uses	TIER 1 Neighborhood 1 Zoning Districts, N2-A, MHP, ML-1, ML-2, IC-1, OFC, OG Zoning Districts		TIER 2 N2-B, N2-C, IMU, IC-2, RC, NC, CAC-1, CG, CR Zoning Districts		TIER 3 CAC-2, TOD-UC, TOD-NC, TOD-CC, TOD-TR, RAC, UC, UE Zoning Districts	
	Minimum	Maximum <i>Tier 1 does not have a parking maximum</i>	Minimum	Maximum <i>Applies to both parking lots and parking structures</i>	Minimum <i>Applies only when within 400' of a Neighborhood 1 Place Type</i>	Maximum <i>Applies to both parking lots and parking structures</i>
Dwelling – Single-Family	2/dwelling unit		1/dwelling unit	No limit on enclosed/garage spaces; 2 unenclosed/dwelling unit		No limit on enclosed/garage spaces; 2 unenclosed/dwelling unit
Dwelling – Triplex	1.5/dwelling unit		1/dwelling unit	No limit on enclosed/garage spaces; 2 unenclosed/dwelling unit		No limit on enclosed/garage spaces; 2 unenclosed/dwelling unit
Dwelling – Quadraplex	1.5/dwelling unit		1/dwelling unit	No limit on enclosed/garage spaces; 2 unenclosed/dwelling unit		No limit on enclosed/garage spaces; 2 unenclosed/dwelling unit
Fraternity/Sorority Facility	1/bedroom		0.5/bedroom	2/bedroom		1/bedroom
Group Home	1/2 residents		1/4 residents	6 spaces		6 spaces
Manufactured Home Park	1/manufactured site		1/manufactured site	2/manufactured site		2/manufactured site
Residential Care Facility	1/bed		0.5/bed	1.5/bed		1/bed
Single Room Occupancy (SRO)	0.2/rooming unit		0.2/rooming unit	1/rooming unit		1/rooming unit

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	Minimum	Maximum <i>Tier 1 does not have a parking maximum</i>	Minimum	Maximum <i>Applies to both parking lots and parking structures</i>	Minimum <i>Applies only when within 400' of a Neighborhood 1 Place Type</i>	Maximum <i>Applies to both parking lots and parking structures</i>
Commercial Uses						
COMMERCIAL USE unless listed below	1/750sf GFA		1/1,000sf GFA	1/250sf GFA		1/250sf GFA
Amusement Facility - Indoor	1/750sf GFA + 50% of outdoor area		1/1,000sf GFA + 50% of outdoor area	1/250sf GFA + 50% of outdoor area		1/250sf GFA + 50% of outdoor area
Amusement Facility - Outdoor	1/1,500sf GFA + outdoor area		1/2,000sf GFA + outdoor area	1/250sf GFA + outdoor area		1/250sf GFA + outdoor area
Bed and Breakfast	1 space + .5/room		1 space + .5/room	1 space + 1/room		1 space + 1/room
Car Wash	1/wash bay		0.5/wash bay	2/wash bay		2/wash bay
Drive-Through Establishment	1 space		1 space	2 spaces		2 spaces
Gas Station	1/pump island + 1/500sf GFA of retail		1/pump island + 1/1,000sf GFA of retail	1/250sf GFA of retail		1/250sf GFA of retail
Greenhouse/Nursery - Wholesale	1/750sf of office area + 1/10,000sf of growing area (indoor + outdoor)		1/1,000sf of office area + 1/10,000sf of growing area (indoor + outdoor)	1/250sf of office area + 1/10,000sf of growing area (indoor + outdoor)		1/250sf of office area + 1/10,000sf of growing area (indoor + outdoor)
Heavy Rental and Service Establishment	1/750sf GFA + 1/10,000sf of outdoor area		1/1,000sf GFA + 1/10,000sf of outdoor area	1/250sf GFA + 1/10,000sf of outdoor area		1/250sf GFA + 1/10,000sf of outdoor area
Heavy Retail Establishment	1/750sf GFA + 1/10,000sf of outdoor area		1/1,000sf GFA + 1/10,000sf of outdoor area	1/250sf GFA + 1/10,000sf of outdoor area		1/250sf GFA + 1/10,000sf of outdoor area
Hotel/Motel	1/guest room		0.5/guest room	1.5/guest room		1.5/guest room
Kennel	1 space		1 space	2 spaces		2 spaces

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	Minimum	Maximum <i>Tier 1 does not have a parking maximum</i>	Minimum	Maximum <i>Applies to both parking lots and parking structures</i>	Minimum <i>Applies only when within 400' of a Neighborhood 1 Place Type</i>	Maximum <i>Applies to both parking lots and parking structures</i>
Live Performance Venue - Indoor	1/750sf GFA + 50% of outdoor area; Unless within 400' of a Neighborhood 1 Place Type, then 1/500sf GFA + 50% of outdoor area		1/1,000sf GFA + 50% of outdoor area; Unless within 400' of a Neighborhood 1 Place Type, then 1/500sf GFA + 50% of outdoor area	1/250sf GFA + 50% of outdoor area	1/500sf GFA + 50% of outdoor area	1/250sf GFA + 50% of outdoor area
Micro-Production of Alcohol	1/750sf GFA + 50% of outdoor area - excludes brewing facilities; Unless within 400' of a Neighborhood 1 Place Type, then 1/500sf GFA + 50% of outdoor area - excludes brewing facilities		1/1,000sf GFA + 50% of outdoor area - excludes brewing facilities; Unless within 400' of a Neighborhood 1 Place Type, then 1/500sf GFA + 50% of outdoor area - excludes brewing facilities	1/250sf GFA + 50% of outdoor area - excludes brewing facilities	1/500sf GFA + 50% of outdoor area - excludes brewing facilities	1/250sf GFA + 50% of outdoor area - excludes brewing facilities
Neighborhood Commercial Establishment <i>Applies to new construction only (See Article 15 for establishment in existing buildings)</i>	1/750sf GFA		1/1,000sf GFA	1/250sf GFA		1/250sf GFA
Nightclub	1/750sf GFA + 50% of outdoor area; Unless within 400' of a Neighborhood 1 Place Type, then 1/500sf GFA + 50% of outdoor area		1/1,000sf GFA + 50% of outdoor area; Unless within 400' of a Neighborhood 1 Place Type, then 1/500sf GFA + 50% of outdoor area	1/250sf GFA + 50% of outdoor area	1/500sf GFA + 50% of outdoor area	1/250sf GFA + 50% of outdoor area
Outdoor Market	1/750sf of lot area for market		1/1,000sf of lot area for market	1/250sf of lot area for market		1/250sf of lot area for market
Raceway/Dragstrip	1/5 persons at persons capacity		1/10 persons at persons capacity	<i>Per conditional zoning</i>		<i>Per conditional zoning</i>

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	Minimum	Maximum <i>Tier 1 does not have a parking maximum</i>	Minimum	Maximum <i>Applies to both parking lots and parking structures</i>	Minimum <i>Applies only when within 400' of a Neighborhood 1 Place Type</i>	Maximum <i>Applies to both parking lots and parking structures</i>
Restaurant/Bar	1/750sf GFA + 50% of outdoor area; Unless within 400' of a Neighborhood 1 Place Type, then 1/500sf GFA + 50% of outdoor area		1/1,000sf GFA + 50% of outdoor area; Unless within 400' of a Neighborhood 1 Place Type, then 1/500sf GFA + 50% of outdoor area	1/250sf GFA + 50% of outdoor area	1/500sf GFA + 50% of outdoor area	1/250sf GFA + 50% of outdoor area
Self-Storage Facility: Climate-Controlled	1/25 storage units		1/40 storage units	1/10 storage units		1/10 storage units
Self-Storage Facility: Outdoor	1/25 storage units		1/40 storage units	1/10 storage units		1/10 storage units
Stadium	1/5 persons at persons capacity		1/10 persons at persons capacity	<i>Per conditional zoning</i>		<i>Per conditional zoning</i>
Vehicle Auction Facility	1/10,000sf of lot area		1/15,000sf of lot area	1/5,000sf of lot area		1/5,000sf of lot area
Vehicle Dealership: Enclosed	1/750sf GFA + 4 per service bay		1/1,000sf GFA + 4 per service bay	1/250sf GFA + 6 per service bay		1/250sf GFA + 6 per service bay
Vehicle Dealership: Outdoor	1/750sf GFA of indoor area + 4 per service bay		1/1,000sf GFA of indoor area + 4 per service bay	1/250sf GFA of indoor area + 6 per service bay		1/250sf GFA of indoor area + 6 per service bay
Vehicle Rental: Enclosed	1/750sf GFA of indoor area - excludes indoor storage of vehicles		1/1,000sf GFA of indoor area - excludes indoor storage of vehicles	1/250sf GFA of indoor area - excludes indoor storage of vehicles		1/250sf GFA of indoor area - excludes indoor storage of vehicles
Vehicle Rental: Outdoor	1/750sf GFA of indoor area		1/1,000sf GFA of indoor area	1/250sf GFA of indoor area		1/250sf GFA of indoor area
Vehicle Repair Facility: Major	4/service bay		2/service bay	6/service bay		6/service bay
Vehicle Repair Facility: Minor	4/service bay		2/service bay	6/service bay		6/service bay

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	Minimum	Maximum <i>Tier 1 does not have a parking maximum</i>	Minimum	Maximum <i>Applies to both parking lots and parking structures</i>	Minimum <i>Applies only when within 400' of a Neighborhood 1 Place Type</i>	Maximum <i>Applies to both parking lots and parking structures</i>
Institutional and Government Uses						
INSTITUTIONAL AND GOVERNMENT USE unless listed below	1/750sf GFA		1/1,000sf GFA	1/300sf GFA		1/300sf GFA
Correctional Facility	1/10,000sf GFA		1/15,000sf GFA	<i>Per conditional zoning</i>		<i>Per conditional zoning</i>
Educational Facility - Pre-School	2/classroom		1/classroom	3/classroom		3/classroom
Educational Facility - Primary or Secondary	2/classroom		1/classroom	3/classroom		3/classroom
Educational Facility - University or College	1/1,000sf GFA		1/2,000sf GFA	1/300sf GFA		1/300sf GFA
Educational Facility - Vocational	1/1,000sf GFA		1/2,000sf GFA	1/300sf GFA		1/300sf GFA
Government Office/Facility	1/750sf GFA		1/1,000sf GFA	1/300sf GFA		1/300sf GFA
Place of Worship	1/4 seats of largest public assembly area		1/8 seats of largest public assembly area	1/6 seats of largest public assembly area		1/8 seats of largest public assembly area
Public Health and Social Service Uses						
PUBLIC HEALTH AND SOCIAL SERVICE USE unless listed below	1/750sf GFA		1/1,000sf GFA	1/250sf GFA		1/250sf GFA
Addiction Treatment Facility, Residential	1/bed		0.5/bed	1.5/bed		1/bed
Alternative Correction Facility	1/bedroom		0.5/bedroom	6 spaces		6 spaces
Children's Home	2 spaces		2 spaces	6 spaces		6 spaces
Domestic Violence Shelter	2 spaces		2 spaces	6 spaces		6 spaces
Food Bank	1/750sf of office area + 1/15,000sf GFA of warehouse		1/1,000sf of office area + 1/15,000sf GFA of warehouse	1/250sf of office area + 1/15,000sf GFA of warehouse		1/250sf of office area + 1/15,000sf GFA of warehouse
Halfway House	0.2/bedroom		0.2/bedroom	6 spaces		6 spaces
Healthcare Institution	2.5/patient room		2/patient room	5/patient room		5/patient room
Homeless Shelter	2 spaces		2 spaces	6 spaces		6 spaces

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	Minimum	Maximum <i>Tier 1 does not have a parking maximum</i>	Minimum	Maximum <i>Applies to both parking lots and parking structures</i>	Minimum <i>Applies only when within 400' of a Neighborhood 1 Place Type</i>	Maximum <i>Applies to both parking lots and parking structures</i>
Industrial Uses						
INDUSTRIAL USE unless listed below	1/1,000sf GFA up to 40,000sf, then 1/2,500sf for additional GFA above 40,000sf		1/2,000sf GFA up to 40,000sf, then 1/4,000sf for additional GFA above 40,000sf	1/500sf GFA up to 40,000sf, then 1/1,250sf for additional GFA above 40,000sf		1/500sf GFA up to 40,000sf, then 1/1,250sf for additional GFA above 40,000sf
Airport						
Airstrip	1 space		1 space	4 spaces		4 spaces
Beneficial Fill Site						
Crematorium	1/750sf GFA		1/1,000sf GFA	1/250sf GFA		1/250sf GFA
Industrial, Craft	1/1,000sf GFA		1/1,500sf GFA	1/250sf GFA		1/250sf GFA
Landfill, Land Clearing & Inert Debris (LCID)						
Movie Studio	1/1,000sf GFA up to 40,000sf, then 1/2,500sf for additional GFA above 40,000sf		1/2,000sf GFA up to 40,000sf, then 1/4,000sf for additional GFA above 40,000sf	<i>Per conditional zoning</i>		<i>Per conditional zoning</i>
Outdoor Storage Yard	1/20,000sf of lot area		1/20,000sf of lot area	1/10,000sf of lot area		1/10,000sf of lot area
Quarry						
Recycling Collection Center	1/750sf of office area		1/1,000sf of office area	1/250sf of office area		1/250sf of office area
Rail Freight Terminal	1/750sf of office area		1/1,000sf of office area	1/250sf of office area		1/1,000sf of office area

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	Minimum	Maximum <i>Tier 1 does not have a parking maximum</i>	Minimum	Maximum <i>Applies to both parking lots and parking structures</i>	Minimum <i>Applies only when within 400' of a Neighborhood 1 Place Type</i>	Maximum <i>Applies to both parking lots and parking structures</i>
Salvage and/or Junk Yard	1/750sf of office area		1/1,000sf of office area	1/250sf of office area		1/250sf of office area
Solar Farm						
Truck Terminal	1/750sf of office area		1/1,000sf of office area	1/250sf of office area		1/250sf of office area
Warehouse and Distribution Center	1/750sf of office area + 1/15,000sf GFA of warehouse		1/1,000sf of office area + 1/15,000sf GFA of warehouse	1/250sf of office area + 1/7,500sf GFA of warehouse		1/250sf of office area + 1/7,500sf GFA of warehouse
Waste Management Facility	1/750sf of office area		1/1,000sf of office area	1/250sf of office area		1/250sf of office area
Wind Farm						

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	Minimum	Maximum <i>Tier 1 does not have a parking maximum</i>	Minimum	Maximum <i>Applies to both parking lots and parking structures</i>	Minimum <i>Applies only when within 400' of a Neighborhood 1 Place Type</i>	Maximum <i>Applies to both parking lots and parking structures</i>
Transportation Uses						
TRANSPORTATION USES unless listed below	<i>(None)</i>		<i>(None)</i>	<i>(None)</i>		<i>(None)</i>
Passenger Terminal	1/2,000sf GFA		1/4,000sf GFA	1/2,000sf GFA		1/2,000sf GFA
Truck Stop	1/500sf GFA of structure +1 truck space/10,000sf of outdoor area					
Vehicle Operations Facility	1/1,000sf GFA up to 40,000sf, then 1/2,500sf for additional GFA above 40,000sf +1/10,000sf of outdoor area		1/2,000sf GFA up to 40,000sf, then 1/4,000sf for additional GFA above 40,000sf +1/10,000sf of outdoor area	1/500sf GFA up to 40,000sf, then 1/1,250sf for additional GFA above 40,000sf +1/5,000sf of outdoor area		1/500sf GFA up to 40,000sf, then 1/1,250sf for additional GFA above 40,000sf +1/5,000sf of outdoor area
Open Space, Recreation, and Agricultural Uses						
OPEN SPACE, RECREATION, AND AGRICULTURAL USES unless listed below	<i>(None)</i>		<i>(None)</i>	<i>(None)</i>		<i>(None)</i>
Agriculture - Industrial Processes	1/1,000sf GFA up to 40,000sf, then 1/2,500sf for additional GFA above 40,000sf		1/2,000sf GFA up to 40,000sf, then 1/4,000sf for additional GFA above 40,000sf	1/500sf GFA up to 40,000sf, then 1/1,250sf for additional GFA above 40,000sf		1/500sf GFA up to 40,000sf, then 1/1,250sf for additional GFA above 40,000sf
Boarding Stables, Commercial	1/4 stalls		1/6 stalls	1/stall		1/stall
Campground	1/4 campsites		1/6 campsites	1/campsite		1/campsite
Cemetery	1/750sf GFA of office, chapel/parlor, and facilities		1/1,000sf GFA of office, chapel/parlor, and facilities	1/250sf GFA of office, chapel/parlor, and facilities		1/250sf GFA of office, chapel/parlor, and facilities

Table 19-1: Vehicle Parking Requirements

Uses	TIER 1 Neighborhood 1 Zoning Districts, N2-A, MHP, ML-1, ML-2, IC-1, OFC, OG Zoning Districts		TIER 2 N2-B, N2-C, IMU, IC-2, RC, NC, CAC-1, CG, CR Zoning Districts		TIER 3 CAC-2, TOD-UC, TOD-NC, TOD-CC, TOD-TR, RAC, UC, UE Zoning Districts	
	Minimum	Maximum <i>Tier 1 does not have a parking maximum</i>	Minimum	Maximum <i>Applies to both parking lots and parking structures</i>	Minimum <i>Applies only when within 400' of a Neighborhood 1 Place Type</i>	Maximum <i>Applies to both parking lots and parking structures</i>
Golf Course	2/hole		1/hole	4/hole		4/hole
Driving Range	1/tee		0.5/tee	2/tee		2/tee
Marina, Commercial	1/4 slips		1/6 slips	1/2 slips		1/2 slips
Private Recreation Club	1/750sf GFA		1/1,000sf GFA	1/250sf GFA		1/250sf GFA
Recreational Vehicle (RV) Park	4 spaces		4 spaces	8 spaces		8 spaces
Infrastructure Uses						
INFRASTRUCTURE USES	<i>(None)</i>		<i>(None)</i>	<i>(None)</i>		<i>(None)</i>
Temporary Uses						
TEMPORARY USES	<i>(None)</i>		<i>(None)</i>	<i>(None)</i>		<i>(None)</i>
Accessory Uses						
ACCESSORY USES unless listed below	<i>(None)</i>		<i>(None)</i>	<i>(None)</i>		<i>(None)</i>
Adult Care Home				1 space		1 space
Childcare Center in Residence				1 space		1 space
Family Childcare Home				1 space		1 space
Rooming House	1/2 rooming units		1/4 rooming units	1/rooming unit		1/rooming unit

19.3 REQUIRED ELECTRIC VEHICLE CHARGING STATIONS

A. Electric vehicle (EV) charging stations are required per Table 19-2: Required EV Charging Stations for:

1. Multi-family stacked dwellings
2. The residential component of mixed-use developments
3. Hotels
4. Parking lots and parking structures as a principal use

B. There are two types of electric vehicle (EV) charging stations required by this article: EV-Capable and EVSE-Installed. The types of electric vehicle (EV) charging stations are defined in Article 2.

Total Number of Provided Off-Street Parking Spaces	EV-Capable Spaces	EVSE-Installed Spaces
0-9 spaces	None	None
10-25 spaces	20% of spaces (rounded up)	None
26-50 spaces	20% of spaces (rounded up)	1 space
More than 50 spaces	20% of spaces (rounded up)	2% of spaces (rounded up)

C. In determining the number of required EV charging stations, when the result contains a fraction, any fraction is counted as one parking space.

D. For the residential component of mixed-use developments, the number of required EV charging stations shall be applied on a one-to-one ratio to the number of residential units in the development. However, where the number of parking spaces in a development is less than the number of residential units, the required EV spaces will be based on the total number of spaces provided.

E. EV charging stations shall only count toward a development's parking maximum if spaces are EV-Capable. EVSE-Installed stations do not count toward parking maximums.

F. Where a parking minimum is required, EVSE-Installed stations shall count as two spaces.

G. Any EVSE-Installed stations provided in addition to the required EVSE-Installed stations may be counted toward the EV-Capable requirement as two EV-Capable stations.

19.4 REQUIRED BICYCLE PARKING

A. When bicycle parking spaces are required by Table 19-3: Bicycle Parking Requirements, such bicycle parking shall be installed when any of the following occurs:

1. New construction of a principal building.
2. Expansion of an existing principal building resulting in a requirement of more than five additional bicycle spaces.
3. Change of use or expansion of an existing use resulting in a requirement of more than five additional bicycle spaces.

B. Of those uses required to provide bicycle spaces, Table 19-3 will indicate that some uses are required to provide long-term spaces. The required number of long-term spaces is a percentage of the required total bicycle spaces. All other required bicycle spaces shall be designed as short-term spaces. Where a cell is blank and shaded, no short-term and/or long-term bicycle parking is required.

C. For uses where bicycle parking is required, a minimum of two short-term bicycle spaces shall be provided. In no case are more than 30 short-term bicycle parking spaces required to be provided.

D. If short-term spaces are capped as per item C above, the number of long-term spaces required shall continue to be calculated based upon the required number of short-term spaces, disregarding the cap. In the case of nonresidential uses, no more than 50 long-term bicycle parking spaces are required to be provided.

E. Long-term bicycle parking is not required in either of the following conditions:

1. The entire nonresidential development has a gross floor area of 5,000 square feet or less. This does not apply to residential development.
2. Five or fewer bicycle spaces are required.

F. Bicycle parking located in the public right-of-way shall be subject to approval by the Charlotte Department of Transportation (CDOT) or the North Carolina Department of Transportation (NCDOT), as applicable. A requirement to provide bicycle parking does not imply that a right-of-way encroachment will be granted.

Table 19-3: Bicycle Parking Requirements		
Uses	Required Bicycle Spaces	Required % Long-Term Bicycle Spaces
Residential Uses		
RESIDENTIAL USES unless listed below	(None required)	(None required)
Dormitory	1/4 dorm rooms	80%
Dwelling – Multi-Family Stacked and Multi-Family Attached When Units Not on Sublots <i>Also applies to residential component of mixed-use development</i>	1/5 dwelling units	80%
Fraternity/Sorority Facility	1/5 bedrooms	80%
Multi-Dwelling Development	<i>As required by dwelling type</i>	<i>As required by dwelling type</i>
Residential Care Facility	1/8 rooms	25%
Rooming House	1/5 rooming units	80%
Single Room Occupancy (SRO)	1/5 rooming units	80%
Commercial Uses		
COMMERCIAL USES unless listed below	1/1,500sf GFA	25%
Amusement Facility - Outdoor	1/3,000sf of lot area	25%
Bed and Breakfast		
Car Wash	1/bay	
Commercial Kitchen	1/3,000sf GFA	25%
Contractor Office with Outdoor Storage	1/1,500sf GFA of office area	
Convention Center	1/3,000sf GFA	25%
Drive-Through Establishment	2 spaces	
Gas Station	1/1,500sf GFA of retail area	25%
Greenhouse/Nursery - Wholesale	1/3,000sf of lot area	25%
Heavy Rental and Service Establishment	1/3,000sf of lot area	25%
Heavy Retail Establishment	1/3,000sf of lot area	25%
Hotel/Motel	1/20 rooms	25%
Kennel		
Neighborhood Commercial Establishment <i>Applies to new construction only (See Article 15 for establishment in existing buildings)</i>	1/1,500sf GFA	25%
Outdoor Market	1/3,000sf of lot area	
Raceway/Dragstrip	<i>Per conditional zoning</i>	<i>Per conditional zoning</i>
Retail Goods: Showroom	1/3,000sf of lot area	25%
Self-Storage Facility: Climate-Controlled	4 spaces	
Self-Storage Facility: Outdoor	4 spaces	
Stadium	<i>Per conditional zoning</i>	<i>Per conditional zoning</i>
Vehicle Auction Facility	4 spaces	

Table 19-3: Bicycle Parking Requirements		
Uses	Required Bicycle Spaces	Required % Long-Term Bicycle Spaces
Vehicle Dealership: Enclosed	4 spaces with no service facilities; 8 spaces with service facilities	
Vehicle Dealership: Outdoor	4 spaces with no service facilities; 8 spaces with service facilities	
Vehicle Rental: Enclosed	4 spaces	
Vehicle Rental: Outdoor	4 spaces	
Vehicle Repair Facility: Major	4 spaces	
Vehicle Repair Facility: Minor	4 spaces	
Institutional and Governmental Uses		
INSTITUTIONAL AND GOVERNMENTAL USES unless listed below	1/1,500sf GFA	25%
Correctional Facility	<i>Per conditional zoning</i>	<i>Per conditional zoning</i>
Educational Facility - Pre-School	1/8 classrooms	25%
Educational Facility - Primary or Secondary	1/4 classrooms	25%
Educational Facility - University or College	1/5,000sf GFA	25%
Educational Facility - Vocational	1/5,000sf GFA	25%
Public Safety Facility	1/5,000sf GFA	25%
Public Works Facility	1/5,000sf GFA	25%
Public Health and Social Service Uses		
PUBLIC HEALTH AND SOCIAL SERVICE USES unless listed below	1/1,500sf GFA	25%
Addiction Treatment Facility, Residential	1/8 rooms	25%
Alternative Correction Facility	1/2 rooming units	50%
Children's Home	1/8 rooms	25%
Domestic Violence Shelter		
Food Bank	1/5,000sf GFA	25%
Halfway House	1/2 rooming units	50%
Healthcare Institution	1/20 beds	25%
Homeless Shelter	1/4 beds	50%
Industrial Uses		
INDUSTRIAL USES unless listed below	(None required)	(None required)
Industrial, Craft	1/1,500sf GFA	25%
Industrial, Light	1/5,000sf GFA	25%
Light Assembly	1/5,000sf GFA	25%
Movie Studio	1/5,000sf GFA	25%
Warehouse and Distribution Center	1/5,000sf GFA	25%
Wholesale Goods Establishment	1/5,000sf GFA	25%
Transportation Uses		
TRANSPORTATION USES unless listed below	(None required)	(None required)
Parking Lot (Principal Use)	1/25 spaces	50%
Parking – Structured Facility (Principal Use)	1/25 spaces	50%
Passenger Terminal	1/5,000sf GFA of terminal building	25%
Open Space, Recreation, and Agricultural Uses		
OPEN SPACE, RECREATION, AND AGRICULTURAL USES unless listed below	(None required)	(None required)
Agricultural - Industrial Processes	1/5,000sf GFA	25%
Boarding Stables, Commercial	1/4 stalls	
Cemetery	4 spaces	
Conservation Area	2 per acre	
Community Garden	2 per acre	
Golf Course	1/2 holes	25%
Driving Range	1/2 tees	25%
Marina, Commercial	1/15 slips	25%
Private Recreation Club	1/1,500sf GFA	25%
Public Park	4 per acre	

Table 19-3: Bicycle Parking Requirements		
Uses	Required Bicycle Spaces	Required % Long-Term Bicycle Spaces
Infrastructure Uses		
INFRASTRUCTURE USES unless listed below	(None required)	(None required)
Temporary Uses		
TEMPORARY USES unless listed below	(None required)	(None required)
Accessory Uses		
ACCESSORY USES unless listed below	(None required)	(None required)

19.5 DESIGN OF VEHICLE PARKING SPACES

- A.** Each required vehicle parking space shall meet the minimum dimensional requirements of the Charlotte Land Development Standards Manual (CLDSM). Each required parking space shall have direct and unrestricted access to a drive aisle that meets the standards of the CLDSM.
- B.** For parking lots and parking structures of 20 spaces or more, up to 25% of required vehicle parking spaces may be designed and designated for compact vehicles per CLDSM standards.
- C.** The use of required off-street vehicle parking spaces for the sale, repair, dismantling, or servicing of any vehicles, equipment, materials, or supplies is prohibited. The sale and display of goods in required off-street vehicle parking spaces is also prohibited unless specifically permitted within the Use Matrix in Article 15.
- D.** For nonresidential uses and multi-family stacked dwellings, access configurations which require backing directly onto a street from a required off-street vehicle parking space are prohibited.

19.6 DESIGN OF SURFACE PARKING AND PARKING LOTS

A. Surface Parking and Parking Lot Location and Configuration

1. Neighborhood 1 Zoning Districts and Neighborhood 2 Zoning Districts

a. Residential Dwellings

i. Single-Family Dwellings on Individual Lots

The following apply to all single-family dwellings on individual lots:

(A) Required parking spaces for single-family dwellings shall be located either on a driveway, on an improved surface parking pad, or in a garage.

(B) All required parking spaces for residential uses, shall be located on the same lot as the use.

ii. Duplex, Triplex, and Quadraplex Dwellings on Individual Lots

The following apply to all duplex, triplex, and quadraplex dwellings on individual lots:

(A) Parking spaces shall be located on a driveway, on a parking pad, or in a garage.

(B) All required parking spaces shall be located on the same lot as the use

(C) Driveways and parking pads shall have a maximum width of 40% of the lot width up to a maximum of 24 feet in width between any part of a street-facing façade and any front lot line. For corner lots, additional driveways and parking pads may be allowed on the side street but shall not exceed the permitted width as determined by the front lot width. This maximum width may be split between driveways and parking pads, but the cumulative width of all driveways and parking pads may not exceed the maximum width permitted along any applicable street frontage.

(D) Driveways and parking pads may exceed 24 feet in width when located beyond the entire street-facing façade.

iii. All Dwellings

(A) All driveways and parking areas/pads shall be improved surfaces, such as concrete, asphalt, or other material commonly used for the parking of vehicles, but not including grass, dirt, or gravel. However, gravel is permitted for single-family dwellings. Alternative types of improved surfaces may be approved by the Zoning Administrator in coordination with the Stormwater Administrator for sites within the water supply watershed protection districts.

(B) All driveways shall have a minimum width of 10 feet.

(C) Driveways and parking pad spaces shall be a minimum of 20 feet in length as measured from the right-of-way, back of sidewalk, or back of a shared use path, whichever is greater.

(D) Garages for individual units shall be set back a minimum of 20 feet or the required zoning district setback, whichever is greater. The 20 foot distance shall be measured from the right-of-way, back of sidewalk, or back of a shared use path, whichever is greater.

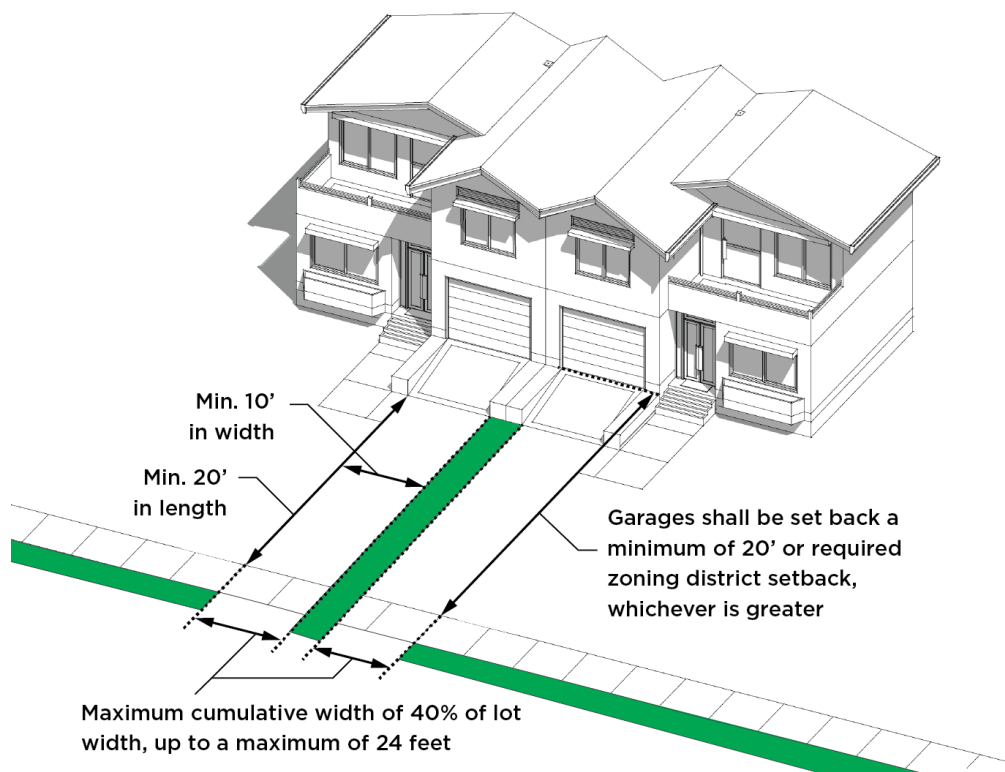
(E) Parking areas other than individual driveways are prohibited within the established front and corner side setback. This does not apply to principal buildings fronting on Limited Access Roads or to single-family dwellings on individual lots.

(F) Driveways shall be as nearly perpendicular to the street frontage as possible. This does not apply to single-family dwellings on individual lots.

(G) Multi-family dwellings and multi-dwelling developments may have a common parking area or areas located within the development site.

(H) Along alleys, driveways and parking pads shall be designed per the CLDSM.

DRIVEWAYS



b. Nonresidential and Mixed-Use Developments

i. All surface parking lots are prohibited in an established setback along a primary frontage. However, the following exceptions apply:

(A) Where there is no principal building, parking areas shall not be located in any required setback or build-to zone, as applicable. In addition, parking areas shall be located a minimum of 20 feet from a right-of-way, back of sidewalk, or back of a shared use path, whichever is greater.

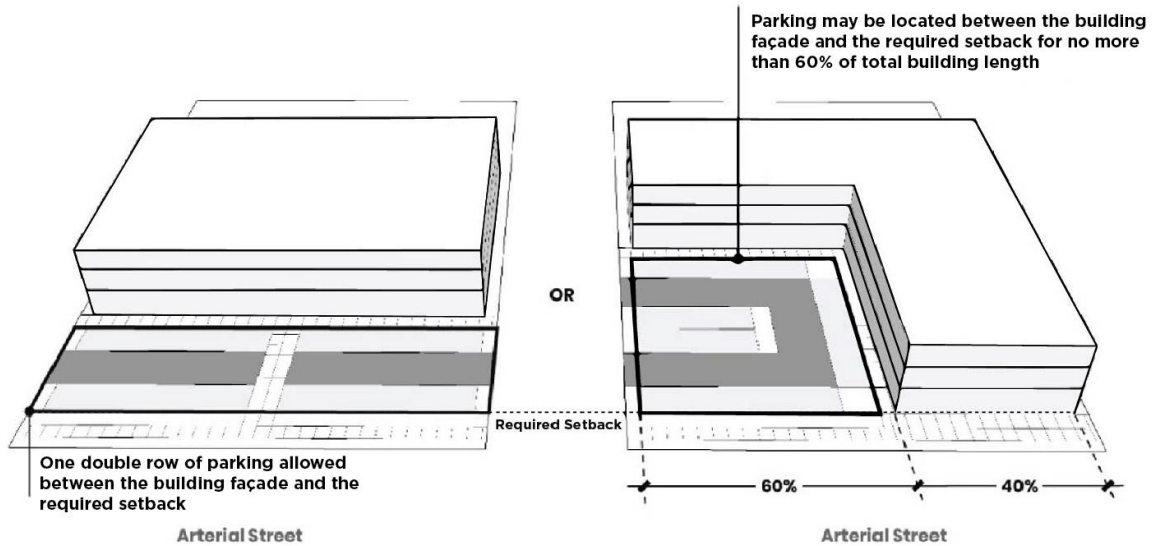
(B) This does not apply to principal buildings fronting on Limited Access Roads; however, such areas are prohibited in the required setback.

(C) For nonresidential development on arterial streets and Parkways in the Neighborhood 1 Zoning Districts, and the N2-A and N2-B Zoning Districts, one double row of parking may be located between the building façade and the required setback.

(D) Alternatively, on arterial streets and Parkways in the Neighborhood 1 Zoning Districts, and the N2-A and N2-B Zoning Districts, parking may be located between the building façade and the required setback for up to 60% of the total building length. The remainder of the building length shall be located closer to the required setback line than the parking.

ii. Parking lots for nonresidential uses shall be located on the same lot as the principal building. Parking lots for nonresidential uses that are part of the development may also be located on a lot that is adjacent to the lot containing the principal building and is used exclusively for parking for that nonresidential use.

ARTERIAL STREET PARKING LOCATION



2. All Other Zoning Districts

a. Surface Parking Design

Surface parking design for all developments are subject to the standards of Table 19-4: Surface Parking Area Location and Access.

b. Additional Surface Parking and Parking Lot Location Standards

i. Residential Uses and Residential Component of Mixed-Use

All required parking spaces for residential uses shall be located on the same lot as the use. Multi-family and multi-dwelling developments may have a common parking area or areas located within the development site.

ii. Nonresidential Uses and Nonresidential Component of Mixed-Use

Parking areas for nonresidential uses and the nonresidential component of mixed-use may be located as follows:

(A) Where there is no principal building, parking areas are prohibited in any required setback or build-to zone, as applicable. In addition, parking areas shall be located a minimum of 20 feet from a right-of-way, back of sidewalk, or a shared use path, whichever is greater.

(B) Within a common parking area or areas of the development site.

(C) In an off-site parking lot or parking structure no more than 800 feet walking distance from the development where the use is located. Such off-site parking shall be approved by the Zoning Administrator. Any off-site parking shall be provided by lease of the off-site parking area for a minimum of five years. Such off-site parking shall be reserved for the exclusive use of the nonresidential use.

(D) For uses where events are held but that are not open for use when no events are occurring, such as stadiums and live performance venues, parking may be located off-site more than 800 feet from the event site. The Zoning Administrator, in conjunction with Charlotte Department of Transportation (CDOT) staff, shall require verification of how the anticipated parking demand will be accommodated, such as by shuttle service. This provision does not apply to places of worship.

Table 19-4: Surface Parking Area Location and Access – Part 1

Standards	Zoning Districts									
	CG	CR	IC-1	IC-2	RC	OFC	OG	ML-1	ML-2	IMU
No surface parking, driveways, circulation, or maneuvering areas shall be located in the established setback along a frontage. This does not apply to a Limited Access frontage; however, such areas are prohibited in the required setback. Driveways shall only be installed across the established setback along a frontage for access to parking areas and shall be as nearly perpendicular to the street frontage as possible.				✓	✓					✓
Surface parking may be located in the established setback along a street; however, such areas are prohibited in the required setback.	✓	✓	✓			✓	✓	✓	✓	
No vehicle travel aisle, including drive aisles for on-site circulation, shall be located in the established setback along a frontage This does not apply to a Limited Access frontage; however, such areas are prohibited in the required setback.				✓	✓					✓
All surface parking along a primary frontage shall be located a minimum of 25 feet behind the setback line and shall also be located behind the established setback. On all other frontages, parking shall be located behind the established setback.				✓	✓					✓
Surface parking located in the established setback along a Main Street frontage shall be removed with any change of use, building addition that exceeds 25% of the gross floor area of the structure or 1,000 square feet, whichever is less, or the addition of 250 square feet or more of outdoor dining area.	✓	✓	✓	✓	✓	✓	✓			✓
No driveways shall be allowed along local and collector streets located across from a Neighborhood 1 Place Type. If all streets are located across from a Neighborhood 1 Place Type, the Zoning Administrator shall approve a means of access that minimizes intrusion into the Neighborhood 1 Place Type.				✓	✓					✓

Table continues on next page

Table 19-4: Surface Parking Area Location and Access – Part 2

Standards	Zoning Districts									
	TOD-UC	TOD-NC	TOD-CC	TOD-TR	NC	CAC-1	CAC-2	RAC	UC	UE
No surface parking, driveways, circulation, or maneuvering areas shall be located in the established setback along a frontage. This does not apply to a Limited Access frontage; however, such areas are prohibited in the required setback. Driveways shall only be installed across the established setback along a frontage for access to parking areas and shall be as nearly perpendicular to the street frontage as possible.	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓
Surface parking may be located in the established setback along a street; however, such areas are prohibited in the required setback.										
No vehicle travel aisle, including drive aisles for on-site circulation, shall be located in the established setback along a frontage. This does not apply to a Limited Access frontage; however, such areas are prohibited in the required setback.	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓
All surface parking along a primary frontage shall be located a minimum of 25 feet behind the setback line and shall also be located behind the established setback. On all other frontages, parking shall be located behind the established setback.	✓	✓	✓	✓			✓	✓	✓	✓
Surface parking located in the established setback along a Main Street frontage shall be removed with any change of use, building addition that exceeds 25% of the gross floor area of the structure or 1,000 square feet, whichever is less, or the addition of 250 square feet or more of outdoor dining area.	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓
No driveways shall be allowed along local and collector streets located across from a Neighborhood 1 Place Type. If all streets are located across from a Neighborhood 1 Place Type, the Zoning Administrator shall approve a means of access that minimizes intrusion into the Neighborhood 1 Place Type.	✓	✓	✓	✓		✓	✓	✓	✓	✓

B. Parking Lot Surfacing

1. All parking lots shall be improved with a hard surfaces, such as concrete, asphalt, or other material commonly used for the parking of vehicles, but not including grass or dirt; gravel is permitted in accordance with item 2 below. Pervious paving is encouraged where appropriate given weight-bearing and traffic requirements.
2. Gravel and other loose material shall be permitted for parking lots, with the exception of driveways that connect directly to a street. Driveways that connect the surface parking lot directly to a street shall meet the following:
 - a. Such driveways shall be improved surfaces, such as concrete, asphalt, or other material commonly used for the parking of vehicles, but not including grass, dirt, or gravel.
 - b. Improved surface driveways shall be a minimum of 20 feet in length as measured from the right-of-way or back of sidewalk or a shared use path, whichever is greater.

C. Striping

All parking lots improved with a hard surface, excluding those improved with gravel, of ten or more spaces shall delineate parking spaces with paint or other permanent materials, which shall be maintained in clearly visible condition.

D. Barriers

1. Barriers, such as wheel blocks, bollards, and curbs, shall be located along the perimeter of parking lots, internal sidewalks and pedestrian connections that abut parking spaces or driveways, and vehicle storage areas, except at ingress, egress, and circulation points. If there is parking on the perimeter of a bus or tractor-trailer lot, bollards, or wheel blocks are required.

2. Such barriers shall be designed and located to prevent parked vehicles from extending beyond designated parking areas.
3. All barriers shall be designed and located in accordance with the standards set out in the CLDSM.

E. Lighting

Parking lots shall be subject to the exterior lighting standards of Section 16.2.

F. Nonconforming Parking Lot and Surface Parking Design

When an existing parking lot of ten or more spaces does not conform to the design requirements of this article, it shall be brought into conformance when the parking lot is fully reconstructed or repaved, or the parking lot area is expanded by greater than 50%. Any expansion of an existing nonconforming parking lot shall be constructed to all applicable UDO standards. Resealing or re-stripping of an existing parking lot, which does not entail paving, resurfacing, or replacement of the asphalt, concrete, or other surface paving material, is not considered reconstruction.

19.7 DESIGN OF PARKING STRUCTURES

A. Applicability of Parking Structure Design Standards

1. All parking structures are subject to the general regulations of item B below.
2. Select zoning districts are subject to the additional standards of item C below and Table 19-5: Parking Structure Design Options when located on the applicable frontage.
3. If there is a conflict between the general regulations in item B and the options outlined in item C, item C shall control.

B. General Parking Structure Design Standards

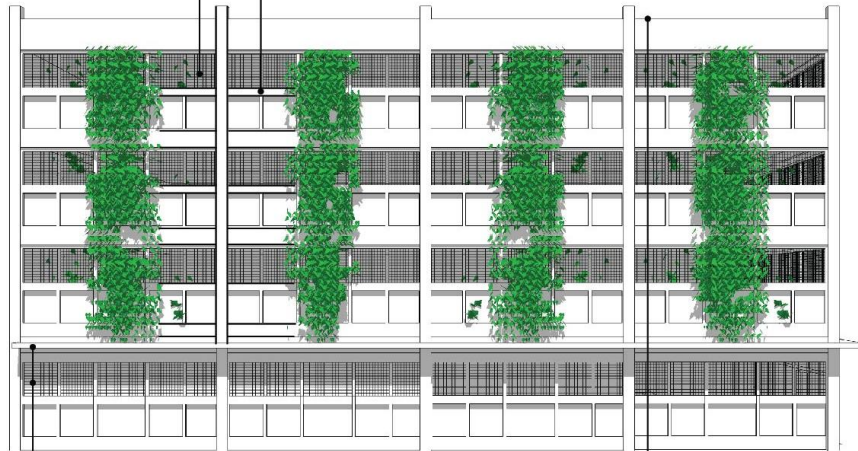
All parking structures are subject to the following standards:

1. On the ground floor of a facade abutting a frontage, where active uses, ingress/egress points, and/or mechanical equipment are not present, pedestrian-scale interest and activity shall be created through the inclusion of at least three architectural elements, such as awnings, overhangs, decorative screens, grills, louvers, pedestrian scale decorative lighting, decorative plantings, or other similar features. This does not apply to Limited Access frontages.
2. Along a frontage, the facades of parking structures shall include both vertical and horizontal treatment that resembles patterns and architecture of the buildings within the development, including use of similar materials and a similar rhythm of window openings. Any openings shall be screened using decorative elements such as grillwork, louvers, green walls, or a similar treatment. This standard shall also apply to a freestanding parking structure that is not part of a larger development, in which case it shall incorporate the patterns and architecture of the surrounding buildings.
3. For parking structures with rooftop open-air parking, a parapet wall a minimum of four feet in height measured from the surface of the roof is required.
4. Facade openings that face any frontage shall be vertically and horizontally aligned.
5. Parking structures shall be designed so that vehicles parked on all levels of the structure and associated lighting are screened by a wall or panel measuring a minimum of 42 inches in height, as measured from the finished surface of the parking level. Along a frontage, the decorative elements indicated in item 2 above shall occupy a minimum of 25% of the area of the opening above the wall or panel.
6. Parking structures shall be subject to the exterior lighting standards of Section 16.2.

GENERAL PARKING STRUCTURE DESIGN

Any openings shall be screened using decorative elements such as grillwork, louvers, green walls, or a similar treatment

Façades shall include both vertical and horizontal treatment to resemble patterns and design of buildings within the development, including use of similar materials and a similar rhythm of window openings.



Pedestrian-scale interest and activity created through the inclusion of architectural elements, such as awnings, overhangs, decorative screens, grills, louvers, etc.

For parking structures with rooftop open-air parking, a parapet wall a minimum of four feet in height measured from the surface of the roof is required.

C. Additional Parking Structure Design Standards by Zoning District

1. Design

- a. Parking structures in select zoning districts shall be designed in accordance with the additional design standards of Table 19-5: Parking Structure Design Options.
- b. Where multiple options are indicated in Table 19-5, any of the indicated options are permitted.
- c. Active use spaces and fully wrapped parking structures are required to meet building articulation and transparency standards of the zoning district.
- d. Parking structures without active use spaces are exempt from the following zoning district standards:
 - i. Building articulation standards for minimum ground floor height and maximum prominent entry spacing.
 - ii. Transparency standards.
- e. The Zoning Administrator may waive the requirement for ground floor activation for parking structures for public transit facilities in constrained conditions when Table 19-5 only allows options with ground floor activation. If such requirements are waived, the parking structure is subject to the general design standards of item B above.

2. Parking Structure Design Options

The options of Table 19-5: Parking Structure Design Options are as follows:

a. Option A - All Floors Wrapped and Ground Floor Activation

- i. All floors wrapped requires a minimum of 70% of the façade above ground floor along any frontage be covered with occupiable building space a minimum of 20 feet in depth. This does not apply for the portion of the facade above the sixth floor.
- ii. Ground floor activation shall meet the standards of Option C below.

b. Option B - Ground Floor Activation and Stepback

- i. Ground floor activation shall meet the standards of Option C below.
- ii. A minimum ten foot building setback is required for any parking located above the ground floor.

c. Option C - Ground Floor Activation

- i. Parking structures shall include residential or nonresidential active uses along 90% of the ground floor building length along any primary frontage and 60% of the ground floor building length along any secondary frontage, excluding areas of vehicular and pedestrian egress, fire stairs, and mechanical or electrical equipment rooms.
- ii. Nonresidential active use bays shall be a minimum of 20 feet in width and 20 feet in depth. Individual spaces shall be furnished with water, sewer, and electrical service, or such services shall be stubbed into each individual active use bay for a future connection.

d. Option D - Additional Setback with Landscape Area

- i. Foundation landscape is required along the entire façade area excluding areas of vehicular and pedestrian egress, and mechanical or electrical equipment rooms. The landscape yard shall count toward any required minimum build-to percentage.
- ii. The width of the additional setback is indicated by the number associated with Option D in Table 19-5. This landscape area setback is in addition to the setback required by the zoning district.
- iii. The landscape area shall be planted in accordance with the requirements of Section 20.8.

Table 19-5: Parking Structure Design Options

KEY:

Section 19.7.C.2 details the design options:

A = Option A (Section 19.7.C.2.a)

B = Option B (Section 19.7.C.2.b)

C = Option C (Section 19.7.C.2.c)

D = Option D (Section 19.7.C.2.d) and the number indicates the width of the additional setback

Frontage	Zoning Districts								
	N2-C	CG	CR	IC-1	IC-2	RC	OFC	OG	IMU
Main Street	A B	A B C	A B C	A B C	A B	A B	A B C	A B C	A B
6 Lane Avenue/Boulevard	A B C D-15'	A B C D-15'	A B C D-15'	A B C D-15'	A B C	A B C	A B C D-15'	A B C D-15'	A B C
4-5 Lane Avenue/Boulevard	A B C D-15'	A B C D-15'	A B C D-15'	A B C D-15'	A B C	A B C	A B C D-15'	A B C D-15'	A B C
2-3 Lane Avenue/Boulevard	A B C D-15'	A B C D-15'	A B C D-15'	A B C D-15'	A B C	A B C	A B C D-15'	A B C D-15'	A B C
Other - Primary	A B C D-15'	A B C D-15'	A B C D-15'	A B C D-15'	A B C	A B C	A B C D-15'	A B C D-15'	A B C
Secondary	A B C D-15'	A B C D-15'	A B C D-15'	A B C D-15'	A B C D-30'	A B C D-30'	A B C D-15'	A B C D-15'	A B C D-30'
Parkway	A B C D-15'	A B C D-15'	A B C D-15'	A B C D-15'	A B C D-30'	A B C D-30'	A B C D-15'	A B C D-15'	A B C D-30'
Limited Access									

Table 19-5: Parking Structure Design Options

KEY:

Section 19.7.C.2 details the design options:

A = Option A (Section 19.7.C.2.a)

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C = Option C (Section 19.7.C.2.c)

D = Option D (Section 19.7.C.2.d) and the number indicates the width of the additional setback

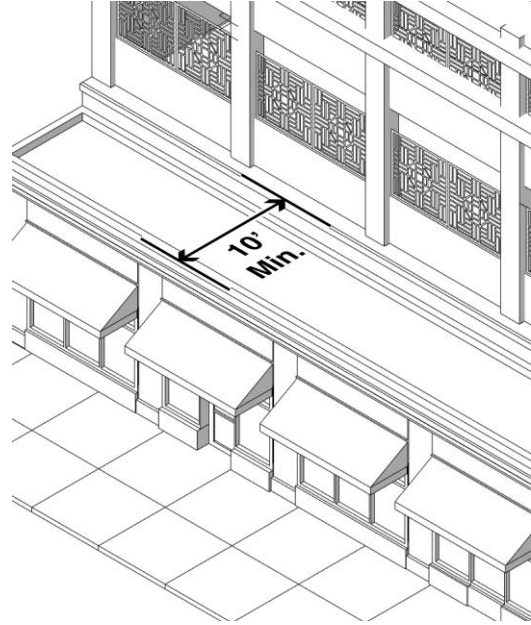
Frontage	Zoning Districts									
	TOD-UC	TOD-NC	TOD-CC	TOD-TR	NC	CAC-1	CAC-2	RAC	UC	UE
Main Street	A	A	A B	A B C	A B	A B	A	A	A	A
6 Lane Avenue/Boulevard	A B C	A B C	A B C	A B C D-30'	A B C D-15'	A B C D-30'	A B C	A B C	A B C	A B C
4-5 Lane Avenue/Boulevard	A B C	A B C	A B C	A B C D-30'	A B C D-15'	A B C D-30'	A B C	A B C	A B C	A B C
2-3 Lane Avenue/Boulevard	A B C	A B C	A B C	A B C	A B C	A B C	A B C	A B C	A B C	A B C
Other - Primary	A B C	A B C	A B C	A B C	A B C	A B C	A B C	A B C	A B C	A B C
Secondary	A B C	A B C	A B C D-30'	A B C D-30'	A B C D-15'	A B C D-30'	A B C	A B C	A B C	A B C
Parkway	A B C D-30'	A B C D-30'	A B C D-30'	A B C D-30'	A B C D-15'	A B C D-30'	A B C D-30'	A B C D-30'	A B C D-30'	A B C D-30'
Limited Access										

PARKING STRUCTURE DESIGN OPTIONS

Option A



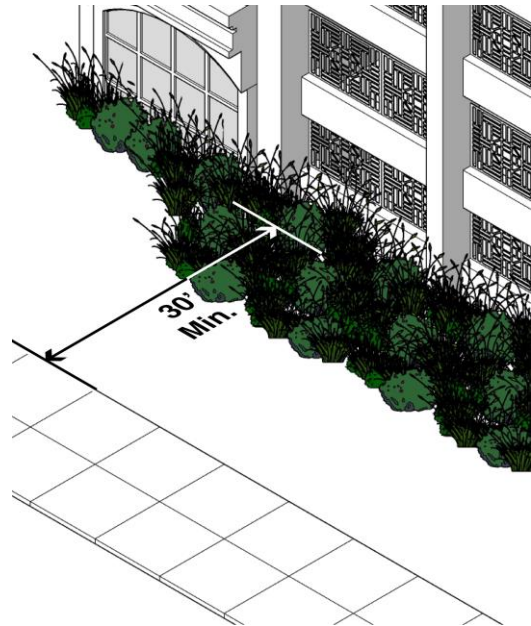
Option B



Option C



Option D



19.8 DESIGN OF UNDERGROUND PARKING STRUCTURES

- A.** All uses are permitted to have an underground parking structure(s). All portions of such structure(s) shall be fully underground, except for ingress/egress points.
- B.** An underground parking structure may encroach into a setback but shall be no closer than five feet from the right-of-way line nor shall encroach into any area reserved for a utility easement, a landscape yard, or green area required by Article 20.

19.9 DESIGN OF BICYCLE PARKING

A. General Standards

- 1. Bike lockers and racks shall be located on a hard surface and be securely anchored.
- 2. All bicycle lockers, bicycle racks, and bicycle parking spaces and areas shall be designed to meet the standards and design specifications of the CLDSM. Alternative bike locker and bike rack designs may be deemed acceptable by CDOT.
- 3. If required bicycle parking is not clearly visible from the entrance to the building, parking structure, transit station, or lot, a sign shall be posted at the primary entrances of these places indicating the location of the parking.
- 4. Bicycle parking facilities shall provide sufficient security from theft and damage. They shall be securely anchored to the ground, shall allow the bicycle to be securely locked, and shall be in a location with sufficient lighting and visibility.

B. Short-Term Bicycle Parking

- 1. Short-term bicycle parking shall be no more than 120 feet from an entrance to the building it is intended to serve.
- 2. Short-term bicycle parking may be located within the public right-of-way and/or within the required setback, subject to the following additional requirements:
 - a.** The short-term bicycle parking shall not obstruct required sidewalks or paths or movement from on-street parking to the required sidewalks or paths, and shall not impact the minimum planting area or spacing requirements for street trees or required screening.
 - b.** Bicycle parking located in the public right-of-way shall be subject to approval by the CDOT or the NCDOT, as applicable.

C. Long-Term Bicycle Parking

- 1. All long-term spaces shall be fully covered and offer protection from the elements. Long-term bicycle parking may consist of indoor parking, racks in garage structures, and/or bicycle lockers or other means which provide coverage of the bicycle.
- 2. Long-term bicycle parking shall be located either internal to the building or behind the building line along a frontage. Such parking may be restricted for the sole use of employees, tenants, residents, or others at the discretion of the property owner or management.
- 3. Spaces within dwelling units or on balconies do not count toward satisfying long-term bicycle parking requirements.

19.10 VALET PARKING REQUIREMENTS

- A. On private property, all outdoor valet drop-off/pickup locations and maneuvering areas shall be located to the side or rear of the principal building. No maneuvering area shall be located in the established setback along a frontage in Neighborhood Center Zoning Districts, Community Activity Center Zoning Districts, Innovation Mixed-Use Zoning Districts, Transit Oriented Development Zoning Districts, and Regional Activity Center Zoning Districts.
- B. Drop-off/pickup locations and related elements such as kiosks and counters for approved valet parking on a public or network-required private street shall not be located in any amenity zone, planting strip, sidewalk, or shared use path.
- C. The valet parking service and associated structures cannot disrupt pedestrian and vehicular traffic.

19.11 COMMERCIAL VEHICLE STORAGE

A. Residential Development

- 1. One light or medium commercial vehicle may be parked overnight at a residence.
- 2. A medium commercial vehicle may only be parked on a clearly delineated driveway or parking area of the residence. Medium commercial vehicles may not be parked overnight on public streets.
- 3. Parking of large commercial vehicles is prohibited, except as permitted in item D below.

B. Mixed-Use Development

Only light and medium commercial vehicle storage is permitted in mixed-use developments. The parking of large commercial vehicles is prohibited, except as permitted in item D below.

C. Nonresidential Development

Parking of light, medium, and large commercial vehicles operated in conjunction with the use established on the site may be stored on-site.

D. Temporary Parking

This section shall not prevent the temporary parking of emergency vehicles, delivery trucks, moving vans, and similar vehicles used for delivery of goods and services, or the parking of commercial vehicles at an active job site or staging area.

19.12 PASSENGER VEHICLE STORAGE, DISPLAY, AND SALE OR TRADE

The following standards shall apply to the storage of unlicensed vehicles, and the display for sale or trade of licensed and unlicensed vehicles, in the N1-A, N1-B, N1-C, N1-D, N1-E, N1-F, N2-A, N2-B, N2-C, and MHP Zoning Districts.

A. Storage of Unlicensed Vehicles

- 1. No more than (2) vehicles that do not have a current, valid, license plate and are not fully enclosed in a permanent structure shall be permitted outside on any premises, provided:
 - a. Vehicles are registered to the occupant of the premises, or
 - b. An immediate family member of the occupant is the record title owner of the vehicle.
- 2. No unlicensed vehicle is permitted outside on any premises if it is not registered to the occupant of the premises, or if an immediate family member of the occupant is not the record title owner of the vehicle.
- 3. Vehicles described in items 1 and 2 above shall not be located within any required setback or buffer required by this Ordinance, or in any street right-of-way except as provided in item B.5 below.

B. Display and Sale or Trade of Licensed or Unlicensed Vehicles

- 1. No more than (2) vehicles, licensed or unlicensed, may be displayed for sale or trade at any time.

2. The display for sale or trade of vehicles as described item 1 above shall not exceed a period of 60 days per vehicle.

3. During a calendar year commencing January 1 and ending December 31:

a. No more than (3) vehicles, licensed or unlicensed, shall be displayed for sale or trade on a premises, and

b. No more than (3) sales or trades of vehicles, licensed or unlicensed, shall occur within this period.

4. No vehicle, licensed or unlicensed, shall be displayed on a premises for sale or trade if it is not registered to the occupant of the premises, or if an immediate family member of the occupant is not the record title owner of the vehicle.

5. Any vehicle, licensed or unlicensed, displayed for sale or trade on a premises may be in an established setback but shall not be within any street right-of-way.

C. All vehicles must also comply with City code, Chapter 10, Article III, "Removal and Disposition of Abandoned Vehicles, Hazardous Vehicles and Junked Motor Vehicles."

19.13 PARKING AND STORAGE OF VEHICLES ON VACANT LOTS

The parking or storage of commercial and passenger vehicles on vacant lots is prohibited unless as part of a use permitted by Table 15-1: Use Matrix. The site shall also meet all applicable development standards of this Ordinance.